

**1.0 APPLICATION DETAILS**

Ref: 20/02632/FUL  
 Location: 72 Waddington Avenue, Coulsdon, CR5 1QN  
 Ward: Old Coulsdon  
 Description: Demolition of existing dwellinghouse; the construction of a block comprising 5 flats to the front and two pairs of semi-detached dwellinghouses to the rear, with associated vehicular access, vehicle and cycle parking, refuse store and landscaping (Amended plans).  
 Drawing Nos: Location Plan, 001/PA/01 Rev H, 001/PA/02 Rev C, 001/PA/03-2, 001/PA/03 Rev E, 001/PA/04 Rev D, 001/PA/05, 001/PA/06 Rev B, S19/7404/01, 20-1009-TPP-C.  
 Agent: Mr Atri Prashad  
 Applicant: Turnbull Land Limited  
 Case Officer: Nathan Pearce

	<b>1 bed</b>	<b>2 bed</b>	<b>3 bed</b>	<b>TOTAL</b>
<b>Existing</b>	0	0	1	1
<b>Proposed</b> (all market housing)	5	0	4	9

<b>Number of car parking spaces</b>	<b>Number of cycle parking spaces</b>
9	12

- 1.1 This application is being reported to Planning Committee in accordance with the following Committee Consideration Criteria:
- Objections above the threshold in the Committee Consideration Criteria have been received
  - The Ward Councillor (Cllr Margaret Bird) made representations in accordance with the Committee Consideration Criteria and requested committee consideration

**2.0 RECOMMENDATION**

- 2.1 That the Planning Committee resolve to GRANT planning permission subject to the completion of a legal agreement to secure the following:
- A financial contribution of £13,500 for sustainable transport improvements and enhancements.
- 2.2 That the Director of Planning and Sustainable Regeneration has delegated authority to negotiate the legal agreement indicated above.

- 2.3 That the Director of Planning and Sustainable Regeneration has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

### **Conditions**

1. Commencement time limit of 3 years
2. Carried out in accordance with the approved drawings and reports

#### Pre-commencement

3. Construction Management Plan and Construction Logistics Plan
4. Construction environmental plan for biodiversity
5. Ecological protection of protected species
6. Submission of a copy of the mitigation licence for badgers
7. Biodiversity mitigation and enhancement measures
8. Submission of SUDS details

#### Prior to above ground floor slab level

9. Materials / details to be submitted
10. Submission of landscaping details (with minimum of 10 trees provided)

#### Pre-occupation

11. Ecological lighting strategy
12. Submission of site waste management plan for refuse collection

#### Compliance

13. Compliance with Tree Protection Plan
14. Carried out in accordance with the approved ecological surveys and reports
15. All windows and openings on the flank elevations of the flatted block and to the upper floors of the houses to be obscure glazed and non-opening below 1.7m from the internal finished floor level.
16. Implementation of car parking as shown on plans
17. Installation of EVCPs at 20% active and 80% passive
18. One house to be M4(3) and remaining houses and ground floor flats to be M4(2)
19. Compliance with energy and water efficiency requirements
20. Compliance with fire statement
21. Any other planning condition(s) considered necessary by the Director of Planning and Sustainable Regeneration

### **Informatives**

1. Granted subject to a Section 106 Agreement
2. Community Infrastructure Levy
3. Code of practice for Construction Sites
4. Highways informative in relation to s278 works required
5. Compliance with Building/Fire Regulations
6. Construction Logistics Informative
7. Any other informative(s) considered necessary by the Director of Planning and Sustainable Regeneration

- 2.4 That, if within 3 months the legal agreement has not been completed, the Director of Planning and Sustainable Regeneration is delegated authority to refuse planning permission.

### 3.0 PROPOSAL AND LOCATION DETAILS

#### Proposal

- 3.1 The proposal is for the demolition of the single-family dwelling house (two-storey) and the redevelopment of the site to provide 4 new dwelling houses in the form of two pairs of semi-detached dwellings at the rear of the site and a three-storey flatted block of 5 flats at the front of the site.
- 3.2 The site would be accessed via the existing highway on Waddington Avenue and a new access road would be incorporated to provide vehicular and pedestrian access to the rear of the site.
- 3.3 The proposal would provide 9 off street car parking spaces, 13 cycle parking spaces with associated landscaping. Each dwellinghouse would have private amenity to the rear. The flats would have private terrace/balconies and a communal amenity space including an area dedicated to child play.
- 3.4 The existing badger sett within the site would be closed and relocated to a new location at the rear of the site, subject to a licence being granted by Natural England.



*Figure 1-Street scene of proposed flatted block fronting Waddington Avenue*

#### Amendments

- 3.5 It should be noted that amended drawings were received during the application period which made alterations to the appearance of the flatted block, and minor alterations to the layout and levels of the rear houses, access road and car parking to better facilitate the use of land levels and location of the badger sett. Additional ecology information was also received. These revisions have sought to address consultee and objector concerns where relevant.

- 3.6 House no.1 has been increased in size (extended towards the boundary with Winchester Close) to comply with M4(3). A formal re-consultation of the application has taken place.
- 3.7 Additional flood mitigation details have also been received, which confirms the position already shown in earlier documents, hence a further re-consultation was not necessary.

### Site and Surroundings

- 3.8 The application site is located on the eastern side of Waddington Avenue and currently comprises one detached dwellinghouse. There is a change in levels on site with the site rising from the front boundary on the west side up to the centre of the site and then dropping off to the east (approximately a 7.4m difference from the front boundary to the highest point on the site). The area is predominantly residential and comprises a number of two-storey semi-detached and single-storey detached dwellings. The area is traditional in terms of the architectural style and is suburban in character. There is a development of houses nearing completion at the adjoining site of 76-80 Waddington Avenue (now known as Winchester Close) which can be seen in the image below.

- The site has a Public Transport Accessibility Level (PTAL) of 1a which is considered to be very poor.
- The site is at high risk of surface water flooding.
- There are no protected trees on the site.
- The site is in the Croydon Downs Tier III Archaeological Priority Area.
- There are no other specific local plan policy designations against the site.



Figure 2-Aerial view of site

## **Planning History**

- 3.9 There are no relevant planning applications associated with the site. The below pre-application and tree works application are relevant.
- 3.10 20/00628/PRE - Demolition of existing property and erection of 4 x1 and 1 x 2 bedroom apartments (front) and 2 x pairs of 3 bedroom semi-detached houses to the rear.
- 3.11 16/04862/TRE – Fell 1 Ash (TPO 58 2007) – Granted 09.11.2016

## **Planning History on adjoining site at 76-80 Waddington Avenue (now known as Winchester Close)**

- 3.12 19/04003/FUL - Construction of a two-storey four-bedroom dwellinghouse to the front, a row of 8 x two-storey semi-detached dwellinghouses (1x 2-bed and 7x3bed) to the rear with associated vehicular access, 15 car parking spaces, refuse refuge and hard and soft landscaping; following demolition of existing bungalow and garages. – Granted 29.01.2020

## **4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION**

- The principle of the intensified residential development is acceptable given the residential character of the surrounding area and the need for housing nationally and locally.
- The proposal provides good quality accommodation for future residents.
- The design and appearance of the development would not harm the character of the surrounding area.
- The proposed landscaping scheme will safeguard and seek to enhance the biodiversity of the site.
- The living conditions of adjoining occupiers would be protected from undue harm.
- The quantity of parking provision and impact upon highway safety and efficiency would on balance be acceptable.
- Sustainability aspects can be controlled by conditions.

## 5.0 CONSULTATIONS

5.1 The views of the planning service are set out below in material considerations

### PLACE SERVICES (Council's ecological advisor)

5.2 Originally objected to the proposal in relation to insufficient ecological information on bats, badgers and reptiles. Following on from detailed discussions between officers, the Council's ecological advisor and the applicant's ecological consultant further details and clarifications were received which was satisfactory for the ecologist to recommend no objection subject to conditions securing biodiversity mitigation and enhancement measures. [Officer Comment: the conditions are recommended to be attached to the planning permission].

## 6.0 LOCAL REPRESENTATION

6.1 The application was initially publicised by letters of notification to neighbouring properties. A renotification of neighbours has been undertaken following the submission of amended plans and a site notice was erected on 22.08.2022. The number of representations received in response to the consultation are as follows:

No of individual responses: 261; Objecting: 259; Supporting: 0 Neutral: 2

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the Material Planning Considerations section of this report.

<b>Objection</b>	<b>Officer comment</b>
<b><i>Character and design</i></b>	
Overdevelopment of the site, Overbearing impact, out of character / not in keeping with the area	Acknowledged and addressed in paragraphs 8.7-8.21
<b><i>Neighbouring amenity impacts</i></b>	
Overlooking and privacy concerns	Acknowledged and addressed in paragraphs 8.29-8.37
Loss of light and outlook	
Noise	
<b><i>Parking and highways</i></b>	
Insufficient parking will lead to overspill parking on surrounding road	Acknowledged and addressed in paragraphs 8.47-8.56
Highway safety concerns	
Difficulty in accessing parking spaces	
<b><i>Other</i></b>	

No improvement to supporting infrastructure	A CIL contribution will be provided.
Impact due to construction traffic	Acknowledged and addressed in paragraph 8.53
Impact on badger sett, wildlife/biodiversity	Acknowledged and addressed in Paragraph 8.43-8.46
Surface water flooding	Acknowledged and addressed in paragraph 8.57-8.59
Impact on trees	Acknowledged and addressed in Paragraph 8.38-8.42
Lack of affordable housing	Acknowledged and addressed in Paragraph 8.6

6.3 The local Ward Councillor, Margaret Bird, objected to the proposed development and referred the planning application to planning committee raising the following concerns:

- Out of keeping with the area
- Character and appearance
- Poor design, impact on streetscene
- Impact on amenity of neighbouring occupiers
- Overbearing impact on neighbours
- Increased flood risk from excess water

## 7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the London Plan (2021), the Croydon Local Plan (2018) and the South London Waste Plan (2012).

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF) (2021). The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay.

7.3 The main planning Policies relevant in the assessment of this application are:

### London Plan (2021):

- D3 Optimising site capacity through the design-led approach
- D4 Delivering good design
- D5 Inclusive design
- D6 Housing quality and standards
- D7 Accessible housing
- D11 Safety, security and resistance to emergency

- D12 Fire Safety
- D14 Noise
- H1 Increasing housing supply
- H2 Small sites
- H10 Housing size mix
- S4 Play and informal recreation
- G5 Urban Greening
- G6 Biodiversity and access to nature
- G7 Trees and woodlands
- S11 Improving air quality
- S12 Minimising greenhouse gas emissions
- S13 Energy infrastructure
- S15 Water infrastructure
- S112 Flood risk management
- S113 Sustainable drainage
- T3 Transport capacity, connectivity and safeguarding
- T4 Assessing and mitigating transport impacts
- T5 Cycling
- T6 Car parking
- T6.1 Residential parking
- T7 Deliveries, servicing and construction
- T9 Funding transport infrastructure through planning

#### Croydon Local Plan (2018):

- SP2 Homes
- DM1 Housing choice for sustainable communities
- SP4 Urban Design and Local Character
- DM10 Design and character
- DM13 Refuse and recycling
- SP6 Environment and Climate Change
- DM23 Development and construction
- DM25 Sustainable drainage systems and reducing floor risk
- DM27 Protecting and Enhancing our Biodiversity
- DM28 Trees
- SP8 Transport and communications
- DM29 Promoting sustainable travel and reducing congestion
- DM30 Car and cycle parking in new development
- DM40 Kenley and Old Coulsdon

#### Supplementary Planning Documents/Guidance

- Section 106 Planning Obligations in Croydon and their relationship to the Community Infrastructure Levy (2019)
- Croydon SPG 12: Landscape Design
- London Housing SPG (Mayor of London, 2016)
- Accessible London: Achieving an Inclusive Environment SPG (Mayor of London, 2014)

- Character and Context SPG (Mayor of London, 2014)
- Sustainable Design and Construction SPG (Mayor of London, 2014)

## **8.0 MATERIAL PLANNING CONSIDERATIONS**

8.1 The main planning issues relevant in the assessment of this application are as follows:

- Principle of development
- Design and impact on the character of the area
- Quality of accommodation
- Impact on neighbouring residential amenity
- Trees, landscaping and ecology
- Access, parking and highway safety
- Flood risk, SuDS and energy efficiency

### **Principle of development**

- 8.2 The existing use of the site is residential (C3) and as such the principle of redeveloping the site for residential purposes is acceptable in land use terms. Policy SP2.1 of the Croydon Local Plan (2018) applies a presumption in favour of development of new homes and Policy SP2.2 states that the Council will seek to deliver 32,890 homes between 2016 and 2036, with 10,060 of said homes being delivered across the borough on windfall sites. London Plan policy D3 encourages incremental densification to achieve a change in densities in the most appropriate way and policy H2 seeks to increase the contribution of small sites to meeting London's housing needs. Given the above, the principle of intensifying the residential use of the existing site is acceptable, subject to the design and impact on the character of the area being acceptable, which is covered below.
- 8.3 Policies SP2.7 and DM1.1 set a strategic target for 30% of all new homes over the plan period to have 3 or more bedrooms and DM1.2 seeks to avoid a net loss of 3-bed family-sized homes in order to ensure that the borough's need for family sized units is met and that a choice of homes is available in the borough. The proposed scheme would provide four 3-bedroom homes, representing a net increase of 3 family sized homes on the site, so this strategic policy requirement is met.
- 8.4 The Croydon Local Plan has identified that some existing residential areas have the capacity to accommodate growth without significant change to its character. This would see the replacement of a single family dwelling house with a housing development that would increase the existing density, massing and footprint.
- 8.5 This approach optimises the development potential of the site, allows for a suitable layout and appreciation of the site context and allows for a considered response. Therefore, the principle of development in terms of land use is acceptable and would be supported in policy terms.

- 8.6 The proposed scheme on the site for 9 units would not trigger affordable housing contributions in line with policy SP2 or London Plan policy H4 or H5.

### **Design and impact on the character of the area**

#### Height, scale, massing and layout

- 8.7 The proposed flatted block to the front of the site would respect the existing building lines of the neighbouring properties which front Waddington Avenue in terms of their orientation and siting.
- 8.8 The height of the proposed flatted block fronting Waddington Avenue is considered to be acceptable and in line with the objectives of the Croydon Local Plan. Local Plan Policy DM10.1 states that new developments should be of high quality and, whilst seeking to achieve a minimum height of 3 storeys, should respect the scale, height and massing of the surrounding area. The height of the development would be two full floors with accommodation in the roof space and would be of a height that would mediate between the heights of the neighbouring properties at 70 and 74 Waddington Avenue, responding to the topographical change along the street. It should be noted that the proposed height of the flatted block would only be 70cm higher than the height of the existing house. Given that the proposed flatted block would achieve a minimum height of 3 storeys whilst respecting the scale, height and massing of the surrounding area it would therefore comply with the aims and objectives of the policy. The dwellings to the rear of the site would also feature two full floors with accommodation in the roof space however given that they are located 70m to the rear of the flatted block and not clearly visible from the public highway, their scale, height and massing is appropriate.
- 8.9 With regards to the layout and siting of the proposed development, it is considered that the siting of the built form is acceptable. The proposals are set away from the neighbouring side boundaries maintaining a separation distance between the proposed dwellings and the existing neighbouring properties, with the proposed flatted block at the front maintaining the existing front building line facing Waddington Avenue. As such the proposed development would ensure that it is consistent with the existing surrounding built form and would thus respect the existing development pattern of the area.
- 8.10 The dwellinghouses at the rear of the site have been positioned such that they appear as a continuation of the row of four semi-detached pairs nearing completion on the adjacent site at 76-80 Waddington Avenue. As such in this instance the principle of back land development can be supported as similar development exists within the immediate vicinity of the site and forms part of the established development pattern which this proposal would complement.
- 8.11 A through route is provided to the rear of the site to facilitate vehicular and pedestrian access to the rear houses. Each of the houses to the rear would benefit from private amenity space in the form of a designated rear garden, whereas the flatted block would be surrounded on both its front and rear sides by soft landscaping, including communal amenity and child play space.

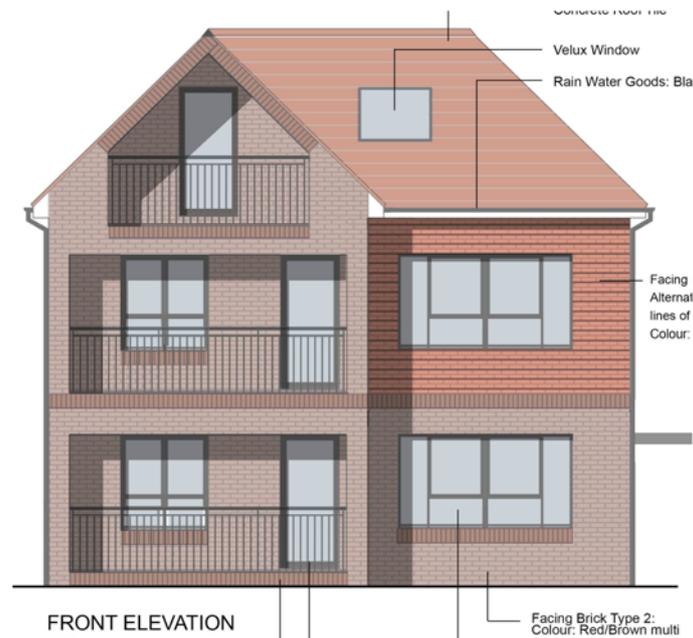


Figure 3-Proposed Site Plan

- 8.12 As previously noted the proposed dwellings to the rear of the site would sit approximately 70m behind the flatted block and circa 100m from the street. There would be a 6.5m gap between the side elevation of the closest property within the Winchester Close development and the closest respective property within the proposed development.
- 8.13 With regard to the front landscaping the proposed layout has an appropriate mix of hard and soft landscaping. This allows for the required quantum of car parking to be provided on-site, whilst allowing meaningful soft landscaping in the form of lawn areas, hedging and trees which would respect and respond to the suburban character of the local area.
- 8.14 There is a change in levels on the site. The land rises from the lowest point at the boundary with Waddington Avenue to high point within the middle of the site which is 7.03m higher. The land then drops by 2m to the rear of the site. The land level of the driveway will be lowered by up to 1.7m from the existing level. A retaining wall will be required for part of the length of the driveway adjacent to 70 Waddington Avenue.
- 8.15 Overall it is considered that the height, massing, scale and site layout of the proposed development would respect the character of the area and thus be in line and consistent with the aims and objectives of Policy DM10 of the Croydon Local Plan.

#### Detailed design

- 8.16 The design principles of the flatted block fronting Waddington Avenue have been drawn from the contextual character analysis to provide an asymmetrical roof form when viewed from the street. A forward projecting gable roof feature would help to break up the massing of the elevation and provides definition so that the building appears as a suitable composition which respects and responds to the wider street scene.



*Figure 4-Elevation fronting Waddington Avenue*

- 8.17 The proposed fenestration of the flatted block to the front of the site has taken a traditional approach in terms of its formation on the front elevation, making it appear as a large house which would be in keeping with the traditional architectural styles of the existing dwellings within the local area. Whilst balconies are proposed these have been inset into the forward projecting gable to minimise their impact upon the streetscene.
- 8.18 The elevation is broken up through the use of different materials at the ground and upper floors. A traditional brick would be used on the ground floor and forward projection of the building and as hanging tiles are a common feature found within the local area, it will be used on the upper floor of the development. A red clay tile would be used for the roof of the dwelling. It is considered that the proposed material choice ensures that the development is sympathetic with the surrounding area in terms of the traditional style of architecture through the use of materials such as brown brick and hanging tiles.
- 8.19 The proposal to the rear takes on a more subdued and subordinate design approach to read as a back land development and to reduce its impact on the proposed and surrounding properties. With regards to the overall built form within the rear of the site, the design has sought to reduce the impact of the built form through the creation of two matching pairs of semi-detached houses. It is considered that this results in a less dominant elevation in terms of the built form and reduces its impact on the character of the wider area, albeit these houses are located circa 100m from Waddington Avenue.



*Figure 5-Front elevations of one pair of dwellings to the rear of the site*

- 8.20 The proposed materials for the rear of the development will complement those to the front to ensure a consistent architectural language across the development.
- 8.21 The design approach is considered in keeping with the character of the local area in terms of its detailed design.

### **Quality of accommodation and internal layout**

8.22 The National Design Guide states that well designed homes should be functional, accessible and sustainable. They should provide internal environments and associated external spaces that support the health and well-being of their users and all who experience them. Homes should meet the needs of a diverse range of users, taking into factors such as ageing population and cultural differences. They should be adequate in size, fit for purpose and adaptable to the changing needs of their occupants over time. London Plan Policy D6 states that housing developments should be of a high quality and provide adequately sized rooms with comfortable and functional layouts. It sets out minimum Gross Internal Area (GIA) standards for new residential developments. This is endorsed by Policy SP2.8 of the Croydon Local Plan. All proposed units comply with the minimum space standards and internal layouts provide hallways and adequate storage space.

<b>Unit</b>	<b>Size (bedroom/ person)</b>	<b>GIA (sqm) proposed</b>	<b>Min. GIA (sqm)</b>	<b>Amenity Space (sqm)</b>	<b>Min. Amenity Space (sqm)</b>	<b>Built in storage space (sqm)</b>	<b>Min. built in storage space (sqm)</b>
Flat 1	1b/2p	50	50	6.7	6	1.8	1.5
Flat 2	1b/2p	50	50	6.7	6	1.8	1.5

Flat 3	1b/2p	50	50	6.7	6	1.8	1.5
Flat 4	1b/2p	50	50	6.7	6	1.8	1.5
Flat 5	1b/2p	63.26	50	13.4	6	3	1.5
House 1	3b/6p	139.9	108	92.9	9	4	2.5
House 2	3b/6p	110.4	108	79.9	9	2.5	2.5
House 3	3b/6p	110.4	108	88.3	9	2.5	2.5
House 4	3b/6p	110.4	108	88.7	9	2.5	2.5

*Table 1: scheme considered against London Plan Policy D6 and Table 3.1*

8.23 The Nationally Described Space Standards (NDSS) provide minimum technical space standards for new dwellings in terms of the gross internal floor areas and storage. All of the proposed units would meet the minimum required gross internal floor area. The proposed houses at the rear are arranged traditionally in terms of their layouts with living accommodation located on the ground floor and bedrooms/study rooms provided on the upper floors. Furthermore, all of the proposed houses would be dual aspect, which will provide suitable cross ventilation, providing greater flexibility in the use of rooms whilst also being better equipped for future adaptability. This would also provide sufficient daylight to enter the units whilst also providing a good level of outlook for the future occupiers of the development. In respect of the proposed flats, 4 of these would be single aspect (either orientated towards the east or west), albeit each flat would have flank, high level and obscurely glazed windows to aid receipt of light. Given the flats have been well designed to avoid deep floorplates, with the hall and bathroom deeper within the plan, they are considered acceptable. The flat within the roof space would be dual aspect.

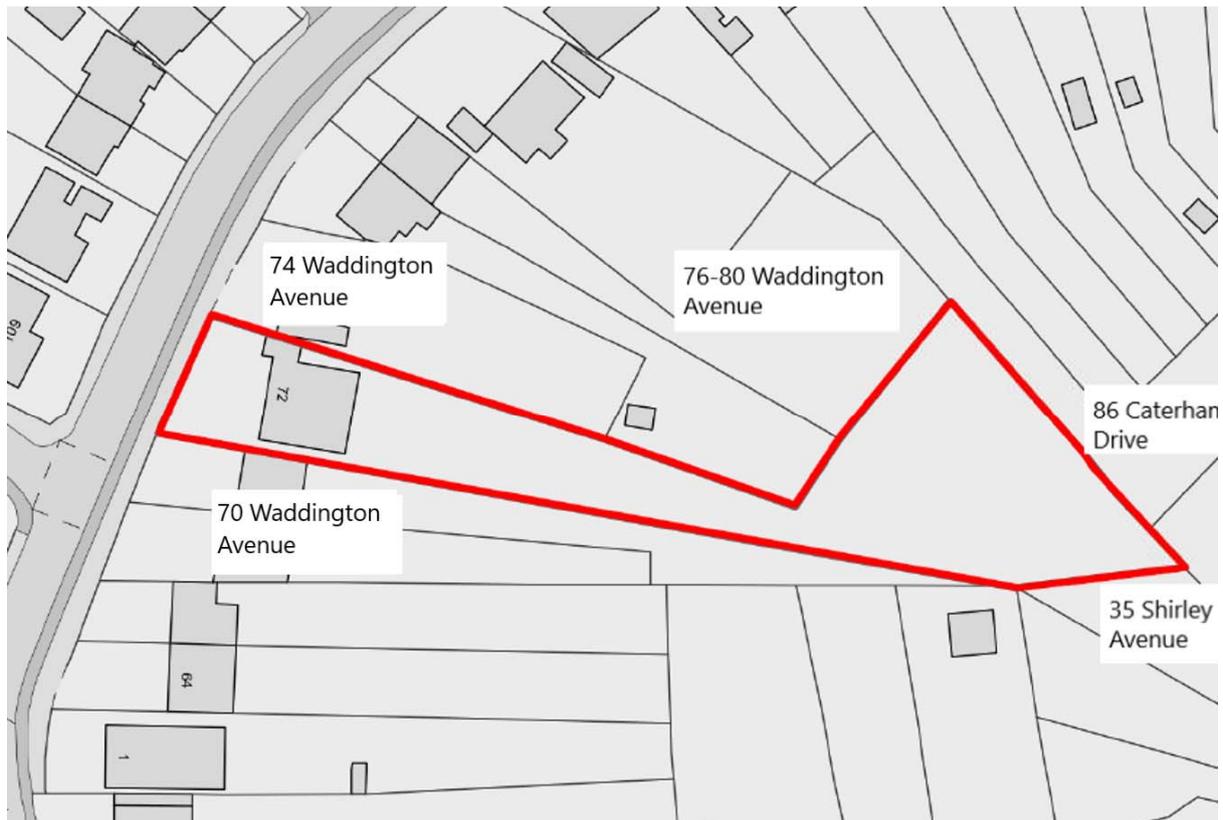
8.24 In respect of private amenity space each flat would have a 6sqm terrace/balcony and the houses would have a minimum garden size of 79m<sup>2</sup> which is acceptable. In addition to the private amenity space provided communal amenity space including 17.5sqm child play provision is provided at the rear of the flatted block which meets the 10sqm policy requirement.

8.25 Good design promotes quality of life for the occupants and users of buildings. In terms of accessibility, one of the houses would appear to be capable of being M4(3) compliant and all of the other houses and ground floor flats M4(2) compliant. These units would be secured by condition, subject to building control approval. The London Plan states that homes not on the ground floor on minor development can comply with M4(1) standard, which does not require step-free access, where provision of step-free access would be unfeasible. The 3 upper floor flats would not be capable of complying with M4(2) due to the absence of a lift; given the small number of units within the upper floors of the flatted block the absence of a lift is accepted by officers as unfeasible in this instance.

- 8.26 London Plan Policy D12 requires that development proposals should achieve the highest standards of fire safety at the earliest possible stage: *'In the interest of fire safety and to ensure the safety of all building users, all development proposals must achieve the highest standards of safety'*
- 8.27 The fire safety statement has been prepared with the level of detail that is appropriate and reasonable for the scale of development. The statement indicates that fire appliances can travel along the access road the rear houses, with the flats served from Waddington Avenue or the access road to the side. The fire appliance can get to within 20m of all units. The properties benefit from gardens and communal amenity/parking areas to the front that would offer easily accessible refuge for occupiers. The buildings have been designed in such a way to minimise the spread of fire as outlined in the Fire Strategy Statement. The proposed development will ensure that there will be viable access for firefighting equipment appropriate for the size of development. It is considered that the statement that has been submitted is sufficient to fulfil the requirements of D12.
- 8.28 Overall the proposal is considered to result in a high-quality development, including an uplift in family accommodation, and will offer future occupiers a good standard of amenity, including the provision of communal amenity space and child play space in accordance with Local Plan Policies SP2 and DM10 and London Plan policies D6, D7 and D12.

#### **Impacts on neighbouring residential amenity**

- 8.29 Policy DM10.6 of the Local Plan states that the Council will ensure proposals protect the amenity of occupiers of adjoining buildings and will not result in direct overlooking into their habitable rooms or private outdoor space and not result in significant loss of existing sunlight or daylight levels.
- 8.30 Representations have been made by the neighbours from adjoining and surrounding properties, with regard to impact on overlooking, outlook, privacy and amenity. Officers have assessed the impact on directly affected neighbours. The properties with the potential to be most affected are the neighbouring properties at numbers 70 and 74 Waddington Avenue.



*Figure 6-Neighbouring Properties (with existing no. 72 property shown)*

70 and 74 Waddington Avenue

- 8.31 The flatted block at the front of the site is situated broadly in the location of the existing dwelling. Based on the submitted drawings, it can be demonstrated that the proposals do not intersect the 45-degree lines in plan when measured from the nearest ground floor neighbouring habitable room windows. No. 70 does not have any flank windows. No. 74 has 2 first floor windows in the flank elevation, these serve a bathroom and a landing, so do not serve habitable rooms. These windows would be separated by 5.5m from the side elevation of the proposed building. Where side facing windows have been proposed these have been indicated as being high level and obscure glazed; a condition requiring all windows and openings on the flank elevations of the proposed flatted block to be obscure glazed and non-opening below 1.7m from the internal floor level has been recommended.
- 8.32 With regards to the proposed height of the building at the front of the site and as noted previously, the built form is arranged over two stories with accommodation within the roof space. It is considered that the proposed height of the building would be acceptable and in accordance with local policy and would therefore have a limited impact on the outlook or the amenity of the neighbouring dwellings.
- 8.33 The dwellings located to the rear would be situated around 70m from the existing dwellings at no's 70 and 74 at an oblique angle and would thus not impact on their amenity.

- 8.34 Given the developments compliance with the Local Plan, overall, the proposals are not considered to result in significant harm to the outlook, privacy and amenity of the neighbouring properties along Waddington Avenue.

Dwellings under construction at 76-80 Waddington Avenue

- 8.35 The rear dwellings would be 6.5m from the side of the dwellings under construction at 76-80 Waddington Avenue and would not break a 45-degree line from the rear or front habitable room windows. Furthermore, a condition requiring all upper floor windows and openings on the flank elevations of the proposed houses to be obscure glazed and non-opening below 1.7m from the internal floor level has been recommended.

82-104 Waddington Avenue; Caterham Drive and Shirley Avenue

- 8.36 The dwellings on Waddington Avenue, Caterham Drive and Shirley Avenue are situated on the roads which surround the rear of the site. These dwellings would be set away from the proposed dwellings to the rear of the site by approximately 60m in distance. Due to the scale, size, design and typology of the rear dwellings, in conjunction with the separation distance, it is considered that there will be minimal harm caused to the outlook and amenity of the neighbouring properties along 82-104 Waddington Avenue, Caterham Drive and Shirley Avenue.

- 8.37 The proposed development would not result in undue noise, light or air pollution uncommon to a residential area, as a result of an increased number of occupants on the site. The increased number of units would increase the number of vehicle movements to and from the site, but this would not be significant and would not be overly harmful. Overall, it is considered that the proposal complies with the requirements of Policy DM10.6 of the Croydon Local Plan.

**Trees, landscaping and ecology**

- 8.38 Policy DM10.8 seeks to retain existing trees and vegetation and policy DM28 requires proposals to incorporate hard and soft landscaping. Policy G7 of the London Plan supports this position.

- 8.39 The site and surrounding land that has been considered contains no category A trees, 2 category B trees, 26 category C trees, 1 category C group and no category U trees. There are 28 trees and 1 group overall as part of tree survey and protection plans. There are 12 trees and 1 group on site with 10 category C trees being removed. There are 16 trees on surrounding land that have been considered and would be safeguarded.

- 8.40 The site provides an opportunity to plant several new trees as part of a comprehensive landscaping scheme as part of the landscaping proposals and full details of replacement tree planting will be required as part of the landscaping condition recommended (with a minimum of 10 new trees proposed to replace those removed). In respect of the existing trees which are to be retained a tree protection plan has been submitted as part of the application and the mitigation and protection measures are considered appropriate in relation to minimising the harm to the health of the existing trees. The Council's tree officer has reviewed

this information and has raised no objection. A condition has been recommended that the development is carried out in accordance with this plan.

- 8.41 The proposed landscaping across the site would consist of a mixture of grassland, shrub areas alongside areas of block paving for the vehicular and pedestrian access routes, as well as terraced planting at the front of the site. It is considered that the landscaping proposals would positively contribute to and reinforce the suburban character of the surrounding area. Overall, the proposed landscaping proposals are considered to provide an appropriate balance between soft and hard landscaping and are thus supported.
- 8.42 Further details on soft landscaping will be required by condition to ensure that the final details and overall quality of the proposals are of an acceptable standard. The tree officer has recommended that the works be undertaken in accordance with the submitted tree protection plan.

#### Ecology

- 8.43 The ecological investigation undertaken found an existing badger sett on site close to the boundary at the rear of what was formally part of no. 76 Waddington Avenue (now part of the adjacent Winchester Close development). This existing sett would be closed and replaced by an artificial sett at the rear of the site surrounded by badger protection fencing and native thorny planting and located adjacent to the boundaries of 35 Shirley Avenue and 88 Caterham Drive. It should be noted that construction of the proposed development cannot commence until the badgers have been suitably relocated from the existing sett to the new sett, and before the existing sett is closed the applicant will be required to apply for a licence from Natural England (who were consulted on this application and raised no objection). Following on from comments raised by the Council's ecological advisor updated documents were received during the application period with regards to the design and location of the replacement artificial sett. Upon further review of said amendments the Council's ecological advisors were content that subject to suitable conditions securing the replacement sett and including the applicant providing a copy of the licence from Natural England, the proposals in respect of the badger sett would be acceptable.
- 8.44 The existing badger sett has one entrance on the neighbouring site at 76-80 Waddington Avenue (now known as Winchester Close). The neighbouring site has been subject of an enforcement enquiry relating to the badger sett entrance, but this has now been resolved so there are no ongoing enforcement cases relating to this badger sett.
- 8.45 The applicant has also provided a full ecological survey of the site including a bat survey and the Council's ecological advisors have assessed said information and are content that the proposals will not negatively impact upon existing biodiversity on site, and that subject to conditions suitable mitigation and biodiversity net gain measures can be secured as part of this development.
- 8.46 Officers are therefore satisfied that the information provided now gives certainty of the likely impacts on protected and priority species and habitats, with appropriate mitigation measures secured. Subject to the recommended

conditions the development is considered acceptable in terms of mitigating the impact to wildlife and seeking to achieve biodiversity net gain.

### **Access, parking and highway safety**

- 8.47 The site has a Public Transport Accessibility Level (PTAL) of 1a which indicates poor access to public transport. Waddington Avenue is however served by a local bus service (route 404) which runs at 30-minute intervals and connects the site to Old Coulsdon, Coulsdon and Caterham-On-The-Hill town centres as well as Coulsdon Town and Coulsdon South stations. It is notable that the local area is hilly, with Waddington Avenue at points being as steep as 1:6.

#### Access arrangements and car parking

- 8.48 The existing property features an 'in and out' driveway arrangement with two crossovers onto Waddington Avenue. The existing southernmost access will be modified and re-used for the vehicle through route to the rear of the site for internal car parking, whilst a new vehicular crossover slightly further south from the existing northernmost access is proposed to allow a single car parking space at the front of the site. The applicant has provided swept path assessments for both accesses using 4.8m long cars as well as a 9.2m long refuse truck for the southernmost access to demonstrate that they would be suitable for their intended purposes. Furthermore, both pedestrian and vehicular sightlines are shown to be achievable within the site for both of the new access points of Waddington Avenue and it has been confirmed that vegetation and boundary treatments will be kept to a maximum height of 0.6m within the visibility splays to ensure adequate visibility.
- 8.49 As previously noted the local area is very hilly with Waddington Avenue having a gradient of up to 1:6. Furthermore the site itself features notable level changes from the front to the rear of circa 7.4m. Whilst all efforts have been made to ensure that the pedestrian and vehicular route into the site is as shallow as possible (with a maximum gradient of 1:8 being proposed), due to the constraints of the site a gradient of 1:12 (as requested by the Council's strategic transport team and as set out in the British Standards (BS8300)) has not been able to be provided in this instance and as such the Council's strategic transport team have objected to the proposed development on this basis. In this specific instance however, officers have had regard to the fact that it is not possible to develop this site with a shallower gradient than that currently being proposed, and given this fact along with the fact that Waddington Avenue is steeper than the proposed access route into the site, as are other private access roads serving other developments within the immediate area (notably Winchester Close), it is on balance considered that the provision of 9 new homes, 4 of which would be family sized, outweighs this issue in the overall planning balance, particularly given the fact that the requirement for a 1:12 gradient is set out in non-statutory guidance, as opposed to the Development Plan itself. Furthermore, it is noted that the blue badge parking space is provided in front of the M4(3) dwelling on a relatively flat portion of the site and that access between the blue badge space and the M4(3) unit would also be relatively flat, in the context of the sloping nature of the area. It is important to stress that in coming to this on balance position, officers have had to weigh up a combination of unique site-specific issues relevant to this site

and as such the acceptance of a steeper gradient in this specific instance should not be seen as a precedent for accepting similar gradients elsewhere.

- 8.50 The scheme proposes 9 on-site car parking spaces, equating to 1 parking space per unit. The proposed car parking provision is considered acceptable for the proportion of family units without leading to a significant overspill of parking on to the public highway.
- 8.51 The *maximum* requirement within the London Plan for a PTAL 1a area would be *up to* 1.5 spaces per dwelling and so, the policy requirement would be a maximum of 13.5 spaces. The proposal is for the provision of 9 spaces. Representations raised concern over inadequate provision of parking spaces for 9 units, however only 4 units would be 3-bed family units, the other 5 units would be 1-bed flats which would likely need fewer parking spaces. Furthermore, as a response to climate change and sustainable development, new developments should not provide excessive car parking, especially given that there is access to public transport provision on Waddington Avenue. Given the above the proposed parking arrangements are acceptable in this instance.
- 8.52 A condition will be attached to ensure 20% active vehicle charging points would be provided in line with policy DM30 and Policy T6.1 of the London Plan, with the remainder passive.
- 8.53 A condition will also be recommended for the submission of a Construction Logistics Plan prior to the commencement of development.
- 8.54 A financial contribution of £13,500 will also be secured through a Section 106 contribution that will go towards improvements to sustainable transport including but not limited to on street car clubs with EVCP's and/or highway changes such as on street restrictions or membership of car club for the units for 3 years. This is required to offset any impacts generated by the development.

#### Cycle parking

- 8.55 Policy DM30 and London Plan policy T5 and Table 10.2 would require provision of a total of 13 cycle parking spaces for residents and given the scale of development, there is no requirement for visitor parking. Cycle parking for the flats will be provided externally between the children's play and the car parking spaces. Each house will have secure external cycle parking in the rear gardens. The overall quantum of cycle parking proposed meets the aforementioned policy requirement and the location and arrangement of the proposed cycle parking is considered acceptable, with final details secured by condition.

#### Refuse / Recycling Facilities

- 8.56 Policy DM13 requires the design of refuse and recycling facilities to be treated as an integral element of the overall design. Given the proposed gradient into the site it will not be possible for the site's refuse to be collected by the Council's refuse contractors and therefore the site will need to be serviced via a private refuse contractor. The applicant has agreed to this and has demonstrated that the site could accommodate a suitably sized refuse vehicle, with a turning head being provided at the rear of the site and tracking drawings demonstrating that such a vehicle could enter and exit the site in forward gear. In this instance such

an arrangement can be deemed acceptable and a condition requiring the applicant to provide the Council with further details of the private refuse contract has been recommended. The houses at the rear of the site would feature external refuse stores within their private gardens whereas the flats will use an external communal refuse store at the front of the site which is approximately 10m from the entrance to the flats and will also allow for the storage of bulky goods. Further details of the refuse stores will be requested as part of the aforementioned condition relating to the private refuse contract.

### **Flood risk, SuDS and energy efficiency**

- 8.57 The site is within flood zone 1 and is at high risk of surface water flooding. A flood risk assessment has been submitted with the application which commits to the use of SUDs across the site in the form of permeable paving and suitable attenuation as close to source as possible such that greenfield run off rates can be achieved. The car park is to be surfaced with permeable paving. The permeable paving system has been modelled in Micro Drainage to accommodate surface water runoff from hard standing areas in up to the 1 in 100 years plus 40% climate change event. The calculations confirm that the paving area could feasibly accommodate the required volume of runoff. The Local Lead Flood Authority have reviewed the details and raised no objection subject to a recommended condition.
- 8.58 A condition requiring details of a Surface Water Drainage Strategy has therefore been recommended and will ensure that the proposals comply with Local Plan policy DM25 and London Plan policy SI13.
- 8.59 In order to ensure that the proposed development will be constructed to high standards of sustainable design in accordance with Local Plan policy SP6, a condition will be attached requiring the proposed development to both achieve the national technical standard for energy efficiency in new homes (2015) which requires a minimum of 19% CO2 reduction beyond the Building Regulations Part L (2013), and meet a minimum water efficiency standard of 110 litres/person/day as set out in Building Regulations Part G.

### **Conclusion**

- 8.62 The principle of the proposed development is acceptable, and the design of the scheme is of an acceptable standard with good quality landscaping which will ensure it complements and respects the existing character of the area. Subject to the provision of suitable conditions the scheme is acceptable in relation to residential amenity, transport, sustainability, and ecological matters.
- 8.63 All other relevant policies and considerations, including the statutory duties set out in the Equalities Act 2010, the Human Rights Act, the Planning and Compulsory Purchase Act, and the Town and Country Planning Act, have been taken into account. Given the consistency of the scheme with the Development Plan and weighing this against all other material planning considerations, the proposal is considered to be acceptable in planning terms subject to the detailed recommendation set out in section 2 (RECOMMENDATION).

8.63 The development would be liable for a charge under the Community Infrastructure Levy (CIL).